



PACIFIC SOUTHWEST DISPATCH

NATIONAL MODEL RAILROAD ASSOCIATION



ARIZONA
CAJON
LOS ANGELES
SAN DIEGO



VOLUME 41, NUMBER 4 4th Quarter, 2023

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I am wishing all of you a very
**Happy Holiday Season and a
Bright New Year!**

Pat Raymer, President





FROM THE WAYCAR

Pat Raymer—President

Time Capsule

Greetings fellow PSR members!

As Chairman of the SurfLiner 2024 NMRA National Convention, I, along with my Co-Chair Joel Morse, have been very busy working with our convention committee to produce an outstanding convention experience for our members.

In the event you are not aware, the last time the NMRA National Convention took place in Southern California was 15 years ago in Anaheim, CA. 12 years prior, the NMRA National Convention was held in Long Beach, CA.

It is that convention, the one in Long Beach 27 years ago, that seems to be recently circling back into my awareness. A couple of months ago I received a wonderful gift in the mail from our good friend and PSR member Gary Burnham. He and his wife Lennie own Tucsonrail Trains and work the train show circuit. You have probably visited with them and not known it. What was the gift? A registrant timetable from the 1996 Long Beach convention!

What a treasure trove of the past! I would say that the majority of model railroaders are history buffs in some fashion, and this document is OUR history.

As I flip through the pages of this timetable, I see clinics presented by such luminaries as Andy Sperandio, Bill Schaumberg, Tony Thompson, Tony Koester, Allen McClelland and many others. The keynote speaker was Gary Coleman of “Different Strokes” television fame. Did you know he was a model railroader? I see advertising from many retailers, some still with us, some long gone. There was even a convention golf tournament!

My next brush with 1996 came from a stack of the NMRA Bulletin magazine (the prior name of the

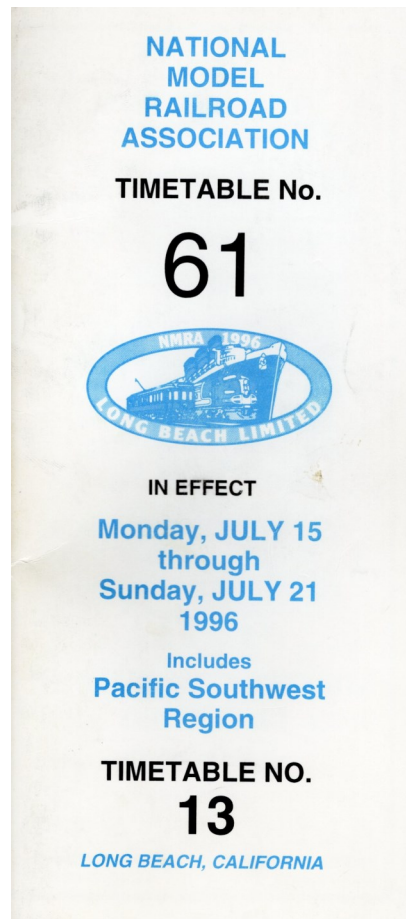
NMRA Magazine) which contained an issue from 1996. There was an article outlining railfanning locations in Long Beach in association with the convention, back when the Southern Pacific was still in business.

These instances got me thinking about a pin collection I won at a Hobo Auction a while back. I brought it out and sure enough, there was a Long Beach Limited 1996 pin. Is this fate, coincidence, or none of the above?

Why, you may ask, am I sharing this trip down memory lane with you? They are not my memories, since I was not a NMRA member in 1996 and did not attend that convention. My reason is simple: it is the shoulders of the people who produced and attended that convention and all of the ones since that we stand upon today as we strive to produce the SurfLiner 2024 NMRA National Convention. We, as the host Region, owe it to them to carry on the tradition and produce an outstanding convention. The best way you can honor

that tradition is to register at SurfLiner2024.org and attend the convention. If the urge strikes you, please volunteer to help. Then, some years from now, you can say “I was there when...”

See you down the line in Long Beach!





THE PSR BACKSHOP

Gary Butts, MMR
Vice President

Merry Christmas and a Happy New Year to you! As I was opening yet another Christmas greeting card from one of our model railroad friends, I had a chance to reflect on just how many new friends we (Sandy and I) have come to know and enjoy through our model railroad hobby. I am late getting our cards out this year because my printer decided to take leave, but I will get them out in time for Christmas. In any case, our lives have been enhanced not only by the enjoyment of the manually and cerebrally creative side of the hobby but the social side of the hobby has become a pretty big part of our lives as well.

We have friends that we know through our other interests but for the most part we see them very occasionally during the boating season or while we are at our place in Baja. No so, with our model railroad friends. We have a group that we operate (trains) with one to two times a month in addition to the NMRA events that happen regularly. We are able to attend most all of the Cajon Division events, some of the LA Division events, all of the PSR region events and some National Conventions and that adds up to one or two events most months. Catching up with these likeminded member friends regarding their latest projects or sharing hobby techniques over an event lunch or dinner keeps us in touch with these folks in a relaxed and creatively stimulating way.

I have come to realize that cementing many of the closer relationships we have has come from my involvement with the administration side of the hobby. This came about in a pretty innocent way by having Per Harwe (then the Cajon Director/Superintendent) suggest that I could advance in the AP program by helping keep track of the Cajon membership. No big deal, I can do spread sheets. Later, when the Order Board Editor position opened up, I fell into that as well. What this did was to put me into the Cajon

Board of Directors and that really opened up the social side of the hobby for me and Sandy as well. I have always been prone to closet style model railroading because grown-ups didn't play with trains but the administration associations I made completely changed this for me.

It turns out that the more involved you are, the more involved you get and it's these involvements that keep life interesting. It is easy to get involved... Just suggest you could help and it will happen in a good way.



CALLING ALL CLINICIANS!

AND THOSE WHO MAY WISH TO
BECOME ONE...

Is there a subject that is near and dear to your heart that you would like to share with others? Would you like to help advance the knowledge of our members? **The SurfLiner 2024 NMRA National Convention is looking for volunteers to present clinics.** Clinic subjects can range from prototype railroads, railroad history, modeling techniques, electronics, scenery, layout design, operations, non-rail subjects, and more than I can list here. Don't be shy-your subject may be just what we are looking for! By the way, if you are participating in the Achievement Program, your clinic, with a handout included, is one of the highest possible author points available! Don't delay-submit your proposal today! Go to www.surfliner2024.org and click on the clinics page to find the clinic proposal form.

Pat Raymer, Chairman

SurfLiner 2024 NMRA National Convention



Cajon

Harold Helland, Director

Hello and Merry Christmas to all of our NMRA PSR members!

I fall into the group that received his first train set under the Christmas Tree on Christmas morning. The tradition is alive and well in our home as our tree is decorated and there is a Lionel train under the tree. My Lionel under the tree includes some original cars with a mix of a couple newer freight cars. I am sure many of you also keep the tradition alive in your homes.

The Cajon Fall Meet was held on October 21, 2023, at the Messiah Lutheran Church courtyard in Buena Park. The weather was perfect, we had 62 members in attendance, and there were five clinics for everyone to learn something new. Between the swap meet and the Hobo Auction there were plenty of opportunities to find something you really wanted for your home model railroad. Once again, several members brought models for show and/or evaluation for merit awards which lead to achievement certificates. A pizza lunch and a chance to renew friendships was enjoyed by everyone.

The Cajon Division is also hosting the Layouts of the Cajon Division. These are home model railroad tours presented by Cajon Division members. The November tour was hosted by Joel Morse on November 18. The January tour will be hosted by David Bestel on January 13, 2024 from 10:00 to 3:00.

Next year, 2024, will be a big year for the Cajon Division. The 2024 NMRA National Convention, SurfLiner 2024 is the responsibility of the Pacific Southwest Region. SurfLiner 2024 will be held at the Long Beach Convention Center August 4 through August 11, 2024, with the base operation at the Westin Hotel. The planning process for the convention is in progress and will be a joint effort of all four PSR Divisions.

Please consider joining the SurfLiner 2024 Convention Team and contributing to the success of the 2024 NMRA National Convention.

As 2023 draws to a close, I hope you have all enjoyed being a member of the Cajon Division which is part of the Pacific Southwest Region of the National Model Railroad Association. For those of you who have participated in our events, I am sure you found items you needed for your model railroad from the swap meets or the Hobo Auction. Many of you learned something new from our clinics or have continued to work on your Achievement Program certificates. For those members who have not participated in our events, I hope in 2024, you will join in the fun and learning experience offered by the Cajon Division events. Your NMRA membership will quickly become more meaningful as you participate and become part of the active Cajon Division members.



Membership

Rudy Spano, Membership Chair

As the year comes to an end, the PSR Membership is still strong. Our October Membership was 943. Our November Membership was also 943. Looking back to our last Dispatch Report, Membership in June was 945 and in July 951. We seem to add a few in one Division and lose a few in another, usually keeping in that 940 to 950 range. Our membership in November 2022 was 943, so we have ended the year where we started.

Looking ahead to the next year and growing our membership, two areas stick out. One is continuing to provide a strong product to our members that they will see as a benefit to their model railroading. Layout tours, clinics online and in person, touting the benefits of the NMRA Website and promoting local and regional events are just a few of the ways we have done this. Are there others we haven't thought about? The other area is promotion. Emails, phone calls, even snail mail to those members without internet to keep them connected, and the continued promotion of events in other Divisions. All of this with the idea of getting members to engage. As Pete said in his last report, Participation makes belonging to the NMRA much more rewarding.

Wishing you all a Happy New Year.



Arizona

Chris Thompson, Director

We are finally experiencing some cooler weather here in central Arizona, and while much of the rest of the US considers this time of year the model railroading season, I'm enjoying getting outside for some fresh air and desert scenery. Of course, with the short days and occasional rainy day, there are still plenty of opportunities to spend time in the train room. That also means the swap meet frequency has picked up across the state and will continue into the spring. There are lots of opportunities to get out and do "train stuff", so hopefully you can find activities to scratch the train itch.

In our monthly Division Zoom meets, in September we had a very good "Ask the MMR" type of clinic with Bruce Petrarca and Peter Youngblood answering questions and engaging in all types of model railroading topics. I always learn something useful in these discussions, so I personally found it educational. In October we had Bill Brown, who is not only the NMRA National Contest Chairman, but also owns his own company called LARC Products. Bill's primary product is a series of CD's containing backdrop scenes broken into a series of files that can be printed on your home computer. The photos are from Bill's massive personal collection he has taken over many years. Each CD contains hundreds of images, and I'm looking forward to trying this out on my layout. Bill is now offering his images on flash drives so you don't need a disk reader to use them.

I was fortunate to be able to attend one of our Sub-Division meets in the Prescott area in November. We got together at a restaurant for a nice lunch and lots of trains conversations, then visited Jim Patterson's very nice BNSF layout set in North Dakota. Jim's layout is 15' by 30' has operating signals and is designed to support operations. I always see things on layouts I visit that could possibly be incorporated into my own layout, and Jim was very gracious to explain several things he did on his layout. I had a great time and hope to be able to make more of those meet ups.

Our November in person meet was in Tucson. A group of folks in the Tucson area who have N-Trak modules brought several of them to the meet and set up a demo layout. The 8' by 20' loop had quite a variety of scenery, and the group had multiple trains running. It was a good opportunity to see what can be done by individuals without a lot space for their own layout, and adding their modules to others makes an impressive set up for running trains.

We did have a little more excitement at the November meet than we really wanted. As we were finishing up lunch, smoke started coming out of a light fixture and air vent in the main meeting room. We evacuated the building and called the fire department. Fortunately, there were no open flames and after about an hour they let us back into the facility. We were able to finish the remaining two clinics scheduled for the afternoon, and complete the contest judging, so we ultimately had a successful meet. After the meet, several of the members in the Tucson area had their layouts open for visits. As mentioned above, I saw a few things for me to think about for my own layout.

Speaking of my own layout, earlier this year I started a new layout in a bedroom that measures 11 ½ feet by 13 ½ feet. The track plan is an around the room with a peninsula coming out from one wall. It will be run as a point to point but have a bridge across the doorway to enable continuous running. I have the benchwork completed on three of the walls, the track is laid and I just finished wiring the track on the third wall. On two of these walls, I have town areas with industry switching, and the wall in between will be mostly scenic area. The peninsula will have a small yard and an engine terminal with a three-bay roundhouse. I will also have a couple staging tracks at each end of the point to point runs. Since the room is small, I will be limiting train length to four or five cars, so the plan is to try to operate with several short trains. I designed the track plan so I will eventually be constructing everything required for the Civil Engineering and Electrical Engineering AP Certifications. I am building all the switches with Fast Tracks tools and materials. After building about 20 of these I tackled a scratch-built crossing. It took awhile to build all the frogs and get them assembled to the NMRA specs, but I managed to get it

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Arizona

Chris Thompson, Director

together and just ran and engine through it since the wiring was completed. I let out a big sigh of relief when the engine traversed the crossing in both directions without any problems! I'm looking forward to starting the scenery on the built sections and constructing the peninsula for the engine terminal.

I grew up in Indiana, so I'm modelling my home state railroad, the Monon. The towns are small rural areas where some of my family came from and feature grain elevators and farming along with a handful of other industries. My goal is to get the feel of these Midwest farming areas in the 60's, so there will be first generation diesels and a lot of boxcars on the layout.

That's it for this quarter and for the year 2023, I hope this has been a good year for you. As Gordy says, be kind to others, not only during this time of year but all year round. I want to wish everyone in the PSR a happy holiday season, and a prosperous and healthy New Year!



Los Angeles

Vic Cavalli, Director

Happy Holidays and wishes for a great New Year. Composing this report during the initial days of December – a quarter which witnessed several high quality events occurring in the Division – instead, I am going to focus entirely on what will be forthcoming during the first months of 2024.

Since my last column, L.A.'s recently elected Superintendent, **Jeff Smith**, has started holding regularly scheduled, monthly meetings of our advisory council. As Jeff puts it, "the opportunities and challenges" that lie ahead should make for a memorable 2024 for the members of our Division.

For example, we will start with Division level elections for the offices of Superintendent, Chief Clerk, and Paymaster in January. Then, announce those results during the Division's annual Membership Meeting and Dinner Banquet, March 9th.

In-between these events, L.A. will hold its next quarterly open house: **Ron Varnell's** 1,400 square foot *Union Pacific* layout on January 21st. *Save the Date!* Not only is Ron's version of the U.P. a great model railroad to view or operate on, but there also will be free hot dogs, deserts, drinks and, of course, lively camaraderie.

Then, the *Great Train Show* will be making its annual stop at the Ventura County Fairgrounds, Ventura, CA, February 17th and 18th. As usual, we should have a recruiting table at the show, so stop by and say "hello".

A busy first quarter that we look forward to prolonging into the second quarter. As Superintendent Smith stated in the Division's latest issue of the *LA Coupler* "...we're looking for new event ideas to bring modelers together. We aim to host [at least] an event each quarter to keep our passion for model railroading alive. If you have suggestions or ideas for such events, please share them with us. Your input is invaluable in shaping the division's calendar." Jeff can be reach at: superintendent@ladiv-nmra.org

I would be amiss if I didn't remind you: the National Model Railroad Association's (NMRA) annual National Convention, *SurfLiner 2024*, and Train Show will be held in Long Beach, CA, August 4th – 11th. Here is your opportunity to experience, first-hand, the best our hobby has to offer: clinics, workshops, superbly built models, layouts, railroad orientated tour, general-interest tours, and more... on a grand scale you might not have ever experienced. (A guarantee: you will not have enough time to attend everything you would have liked to.) **Mark Your Calendars Now! Make Your Reservations Now!**

In closing, don't forget about our L.A. Division's on-line resources, available to all NMRA members:

Sand House Chat "Zoom" Sessions held the first Thursday of each month – contact me at VicCavalli@comcast.net for login details.

Quarterly publication of our newsletter, the *LA Coupler*— see www.ladiv-nmra.org/news-and-information/la-coupler-newsletter/la-coupler-archive for the most current (and back) issue.

Constantly updated/added to website www.ladiv-nmra.org.

Until next quarter, Happy Model Railroading!





San Diego

Tim Foote, Director

I have been busy putting together my holiday train for the living room. I have also done some traveling including a trip to Dollywood in Pigeon Forge, Tennessee. They have an amazing narrow gauge prototype size railroad there that runs into the foothills of the Smokie Mountains. The 2-8-2 Mikado engine (depicted ready to run below) was obtained from the White Pass & Yukon Route at Skagway, Alaska. It burns five tons of coal each day which is shoveled into the firebox by hand. They do their best to maintain the authenticity of the train. Dollywood has a number of other narrow gauge steam engines in various stages of repair.



Every year the San Diego Division holds a meet at the San Diego Model Railroad Museum in Balboa Park. This year we had two excellent clinic presentations. Ken Clark is an expert on repairing brass locomotives. He discussed working with brass, including soldering, cutting and filing, drilling, tools and techniques and also repowering, and maintenance of brass engines. For you brass locomotive fans Ken has an informative web page at shastasprings.com.

Bob Chaparro gave a presentation on right of way & trackside ideas to add realism to your model railroad. Bob has an excellent selection of photos that he presented to inspire one to add more details to their layout. Examples included track maintenance yards, items commonly stored there, freight car maintenance areas (RIP yards), structures along rail lines, railroad phone shacks, battery boxes, switch control boxes, etc. You can find pictures of a lot of these items through an online search and it always helps to see the real thing to inspire your modeling.

Other activities that the San Diego Division has participated in include a visit to the North County Model Railroad Society and Swap Meet in Oceanside. The NCMRS has an excellent and extensive layout at a park facility there. It always amazes me how much progress they have made each year that we visit. They have public operating hours if you would like to visit (check out their web site at ncmrs.org).

In September we took a tour of Mike Komo's Santa Fe Railway Needles District N scale layout. In December the San Diego Division has a holiday party including a layout tour of Don Fowler's (MMR) rebuilt N scale layout of the Southern Pacific's Mojave Division (picture below). It includes the Tehachapi Loop and Southern Pacific's West Colton's Classification Yard, complete with a diesel service facility, and an arrival and departure yard. The layout takes up the entire space of a two car garage which is a large layout for N scale. Parts of the layout are two level and it is all meticulously detailed. I don't know what Don is going to do when he gets it completed, which may not be that far off!





Awards Program

Carl Heimberger, AP Chairman

2023 PSR Awards Fourth Quarter

The PSR had a fairly active fourth quarter with seven achievement awards being issued. Congratulation to the following members

Richard Hock – Structures
 Per Harwe – Chief Dispatcher
 Joel Morse – Chief Dispatcher
 Chris Thompson – Volunteer
 Robert Behm – Scenery
 Michael Guinn – Electrical Engineer
 Michael Guinn - Scenery

If you are working on your MMR, you will need one of the following certificates in order to meet the requirements, official, author or volunteer. For official you are required to serve in some capacity on a national, regional or division board. So, only a limited few can earn this in any one year. Author is not all that hard especially if you love to write articles or give clinics with handouts. Of the three volunteer is probably the easiest in that you just need to earn sixty time units. And you earn them by just volunteering to help with programs put on by your local division, region or even national events.

Why am I brining this up well for the Pacific Southwest Region the next nine months offers our members an excellent opportunity to earn lots of volunteer time units. If you are not aware the 2024 NMRA National Convention the 2024 SurfLiner will be held in Long Beach and there are numerous opportunities for things you can volunteer to do. Here are just a few of the options, clinic room host, bus tour captain, contest room

attendant, company store clerk, information desk, give a clinic, show your home or club layout, the possibilities are almost endless.

So, give it some thought, although you cannot earn all your volunteer time units from a single convention you can earn at least half, and I bet if you really think hard about you have already earned some time units just get them documented before you forget you have them.

If you have questions about the program check with your division AP Manager, they are Gary Butts - Cajon and Los Angeles divisions, Bruce Petrarca – Arizona, and Rodger Gredvig – San Diego. And of course, you can always contact me a directly if I do not have an answer, I will get a clarification from national.

Carl Heimberger



The Show Room

Mike Allee, PSR Contest Chair

In this "time of giving" we usually think of gifts. Physical gifts, but there are other kinds of gifts as well. Giving your time is an important gift to give. The list where time can be given is endless and come in all sorts of amounts of time, from collection plates at church services to daily commitments at schools. The giving of time is volunteering.

What really is volunteering? The answer is a simple one, giving freely of your time in helping others.

The need for many helping hands at a convention is great. A national convention requires lots of volunteers. Volunteers for a national convention such as the NMRA SurfLiner is great. Volunteering requires a commitment that sometimes is hard to keep when you've paid to attend the convention. There are all these things to enjoy; clinics, layout tours, prototype

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The Show Room

Mike Allee, PSR Contest Chair

Continued from page 8

tours, operation sessions and just the venues that come with the area. The NMRA has not only provided a convention to learn and enjoy, as well as providing uniformity to the hobby, educational materials, and an opportunity to enjoy the company of other modelers. It takes people to provide all of this. The NMRA could provide for all of this by paying people. The only problem is you'd I wouldn't attend the convention because the cost would be prohibitive.

Volunteering is were you can pay for these things that are provided and aren't paid for with currency. Commit too giving a little time so we all can enjoy the hobby just a little more. The time you volunteer will not be wasted not enjoying the convention but rather enjoying the convention in ways you haven't thought of until you have the experience. You're involvement could be, being the bus captain on a tour (that you wanted to go on anyway), handling some portion of registration (greeting/helping people is enjoyable), introduce a clinic speaker (accidentally learn something about the hobby you didn't know about and gives you a new area of the hobby to enjoy), play an active part in the company store (getting to meet people and have a conversation), be a contest judge (a great way to learn the ins-and-outs of how really easy it is to build and record on a quality model to show), participate in the security of the Celebration (contest) Room (meeting fellow modelers). The list goes on.

There is a reward beyond those mentioned above. You would receive volunteer Time Unit (TU) credit toward a Volunteer Certificate. Might not mean much now but down the road when the Master Model Railroader MMR desire bites you you'll have some credit towards it from a time you thought it wouldn't be of any value. You were just volunteering out of the goodness of your heart.

Volunteer for the SurfLiner NMRA's 2024 convention in Long Beach Aug. 11- 17, 2024



Historian's Report

Per Harwe, Historian

Per has indicated that he has not received any new historical information and is hoping that members remember to provide Photos, Pins and Badges of past events to him for our archive. - Ed.



PSR Treasurer

Joel Morse, Treasurer

TREASURERS WINTER 2023 DISPATCH REPORT

During the period September 1, 2023, through November 30, 2023, there were several PSR financial transactions, both deposits and credits.

On the Credit (expense) Side, National Membership Rebates to the Divisions totaling \$836.00, as well as payment for the 1,500 "NMRA Join the Fun" labels for the Union Station event and Division events membership recruiting events (\$406.22), a new annual fee for the PSR Zoom Account (\$156.65), reimbursement to Gary Butts for PSR Award Plaques (\$282.35), Hosting Fee for the Arizona Division Website (\$35.98) and other miscellaneous office/supply charges (\$43.20).

On the Deposit Side, only the NMRA National Membership Rebates (\$836) were deposited in this period.

The November 30, 2023, Bank Statement indicates that the Region has \$21,508.69 in the Region Checking Account.

Note that the PSR loaned \$10,000 in "seed money" to the 2024 Convention Committee, which remains outstanding.

PSR Wells Fargo Checking Account:

Region Assets as of September 1, 2023: \$22,258.09

Region Assets as of November 30, 2023: \$21,508.99

Net Change this period: - \$749.40.

Small Layout Operations

Gary Butts, MMR

A few of years ago my wife, Sandy, and I discussed expanding our small layout into something a little larger. This may not seem to be a big deal, but we live in Southern California and basements are essentially non-existent. Our layout resides in our homes' "Living Room". Many years ago, we noted that this front room was rarely used or even visited. Furniture located there required dusting but never cleaning. Our guests, like most I expect, gravitate immediately to our kitchen/family room for conversation and camaraderie with whoever is cooking or watching the TV. So, why not build a small layout in that front room? And so, years ago, we did. It was/is a packed 4' x 8' freelanced HO scale layout set in the late 1800's to early 1900's designed for constant operation on DC.

Fast forward to the layout expansion. Clearing out the remaining unused furniture in the front room and adopting a "U" shape layout shape grew the layout to its current 11' x 15' footprint. We added to our elevated rails by running them around the outside of the new layout section giving us an opportunity to detail out a small logging transfer operation being fed by a log flume and another town-site was established at the base level. Available on the new section was room for a small switching yard and a few industrial stubs. When DCC became commercially available, converting the layout was a snap. By turning all of the DC block switches on and adding a double pole switch to the power feed we can now operate either DC or DCC.



Layout overhead: Left to Right: Pat Raymer (President, Pacific Southwest Region), Sandy Butts, Steve Cole, Brad Joseph (visiting operator from St. Louis), Mona Raymer, Gary Stenberg.

Small Layout Operations, Continued

What about running operations on our little layout? Following Cajon Division Superintendent Joel Morse’s lead and suggestions regarding having a purpose for your railroad’s existence, we realized that, by accident, we had the start of that situation going on our layout. With the addition of a couple of reciprocal industries like a small coal and ore mine serving a coal and ore distributor, a logging transfer operation supplying a lumber yard, a couple of towns with passenger stations, we had the basics in a very compact world. By adding a couple of freight docks and a team loading dock, we were set. Of course, we needed the rolling stock to support these industries and our modeling then took on a more directed purpose. Although the layout is small, it is compact. With over 140 feet of track, not counting stubs, the layout could easily accommodate multiple short trains consisting of a loco and 3-8 cars each. The question was: Could operations be realistically held on this small a layout?

The answer was Yes! they could and we do. We usually run 3-4 simultaneous trains and a yard job, usually each train in an “out and back” scenario but at least one interchange train is brought into the new yard for classification and sent back out on its way. Typically, we spice it up with a passenger or president’s inspection extra winding its way through the whole layout with high priority. The newly expanded section of the layout is somewhat simpler in its track plan than the original 4 x 8 in that the original section contains two inherent reverse loops inside of two main loops located around the outside of the layout with an elevated section in the middle, a four-track maintenance yard, long and short sidings and a couple of double crossovers. For a peek at our original pre-expansion, layout,

visit NMRA’s “NMRA At Home” article on our Gary and Sandy Railroad on the internet at: <https://www.nmra.org/sites/default/files/sr201504tour2a.pdf>

**GARY & SANDY RAILROAD
&
K. J. LUMBER EXTENSION**

TIME SCHEDULE No. 1
TO TAKE EFFECT
WEDNESDAY, NOVEMBER 2, 1921
FOR THE GOVERNMENT AND INFORMATION OF EMPLOYEES ONLY

| TSORO ROUTE | | A.M. TRAINS | | MIDWAY ROUTE | |
|-----------------|------------------|---------------------------|-------------------------|---------------------------|------------|
| #1025 W. LUMBER | #1027 W. FREIGHT | MILES FROM G&S MAINT. YD. | NAMES OF STATIONS | MILES FROM G&S MAINT. YD. | #1030 COAL |
| 8:30 am | | 0.0 | G & S MAINTAINENCE YARD | | |
| 8:35 am | | 0.8 | MIDWAY | | |
| 8:40 am | 8:00 am | 1.8 | G & S INTERCHANGE YARD | K. J. LUMBER | 8:40 am |
| 8:50 am | 8:10 am | 3.8 | TSORO YARD | SANDY COAL & ORE | 8:50 am |
| 9:15 am | | 4.3 | TSORO TUNNEL W. | K. J. LUMBER | 9:40 am |
| 9:20 am | | 6.2 | CHIP'S LUMBER | SANDY COAL & ORE | 10:30 am |
| 10:00 am | | 6.3 | TSORO STATION | ORANGE GROVE | 10:35 am |
| 10:30 am | | 8.0 | FURLOW FREIGHT | CHANTEY SPRINGS STATION | 10:40 am |
| 10:40 am | | 11.6 | CHANTEY SPRINGS STATION | HOBO CAMP | 10:45 am |
| 10:55 am | | 12.6 | ORANGE GROVE | LADY B. LOW CUTOFF | 10:50 am |
| 11:05 am | | 14.2 | SANDY COAL & ORE | MIDWAY SIDING | 10:55 am |
| 11:10 am | | 15.2 | K. J. LUMBER | MIDWAY COAL & ORE | 11:00 am |

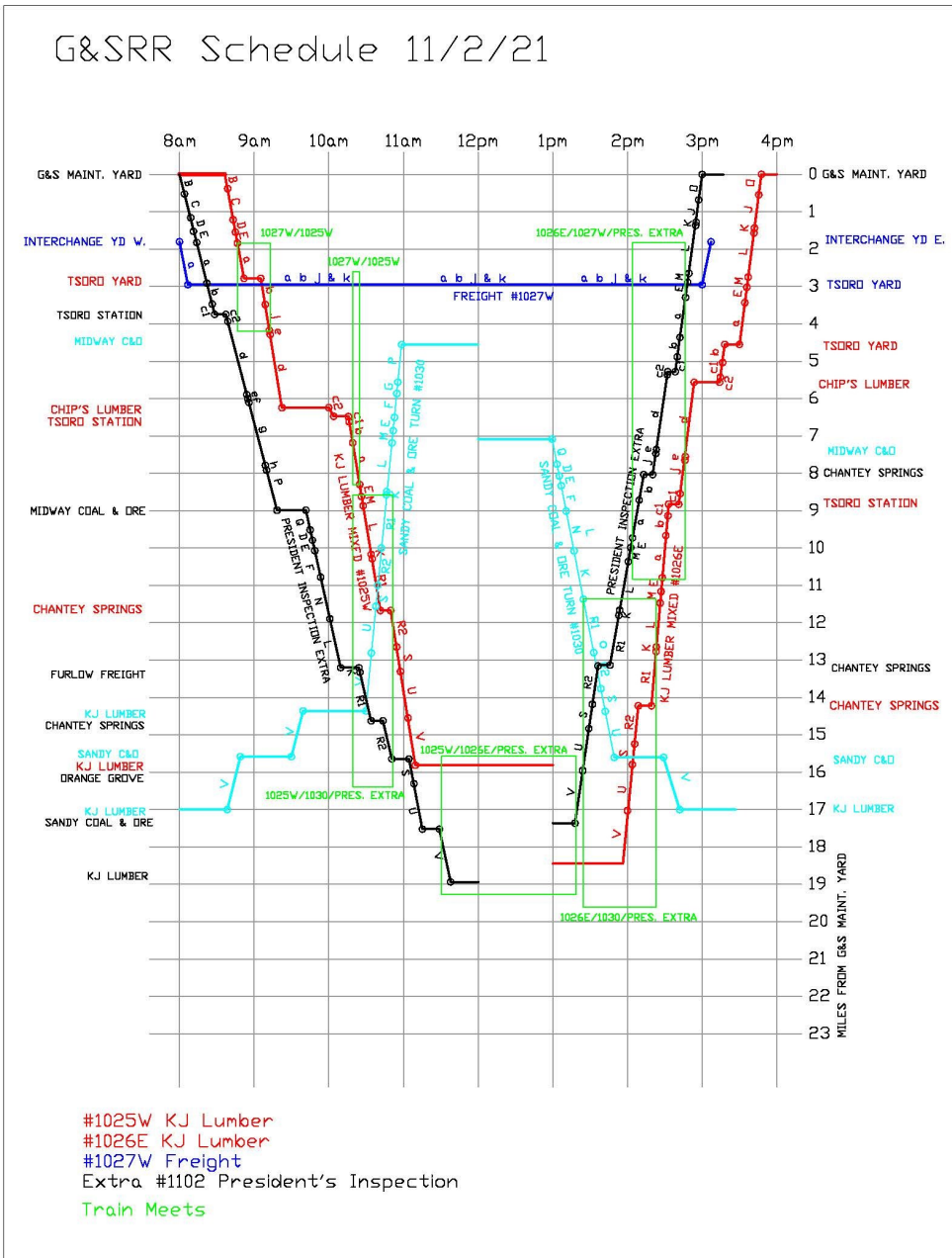
| TSORO ROUTE | | P.M. TRAINS | | MIDWAY ROUTE | |
|-----------------|------------------|---------------------------|-------------------------|---------------------------|------------|
| #1026 E. LUMBER | #1027 W. FREIGHT | MILES FROM G&S MAINT. YD. | NAMES OF STATIONS | MILES FROM G&S MAINT. YD. | #1030 COAL |
| 1:55 pm | | 18.4 | K. J. LUMBER | MIDWAY COAL & ORE | 4.5 |
| 2:05 pm | | 17.4 | SANDY COAL & ORE | G & S INTERCHANGE YARD | 5.6 |
| 2:10 pm | | 15.8 | ORANGE GROVE | LADY B. LOW CUTOFF | 6.3 |
| 2:15 pm | | 14.8 | CHANTEY SPRINGS STATION | CHANTEY BRIDGE WEST | 9.0 |
| 2:25 pm | | 11.8 | FURLOW FREIGHT | HOBO CAMP | 10.8 |
| 2:30 pm | | 11.3 | G & S INTERCHANGE YARD | CHANTEY SPRINGS STATION | 12.8 |
| 2:40 pm | | 8.8 | TSORO STATION | ORANGE GROVE | 11.8 |
| 2:50 pm | | 7.6 | TSORO TUNNEL W. | SANDY COAL & ORE | 14.2 |
| 2:55 pm | | 5.6 | CHIP'S LUMBER | K. J. LUMBER | 17.0 |
| 3:15 pm | 3:00 pm | 4.5 | TSORO YARD | | |
| 3:40 pm | 3:10 pm | 3.0 | G & S INTERCHANGE YARD | | |
| 3:50 pm | | 0.0 | G & S MAINTAINENCE YARD | | |

To keep our trains running smoothly and more or less on time, all of our trains run on schedules based around train orders and switch lists under Dispatcher Direct Traffic Control (DTC) verbal track block warrants. We operate our schedules against a 4:1 fast clock and most of our trains are designed around completion in an 8 to 9 hour day which results in sessions running around 2 hours, sometimes stretching to 3.

We usually operate with 4 operators (sometimes we will double up a switchman or conductor) at the same time and it can get pretty crowded inside of the usually roomy “U” section of the layout. If we have an additional guest, and they are willing, we will station them in the back of the original layout tunnel section at the tunnel staging and crossover as that section’s towerman. His job is to ensure that the hidden tunnel turn-outs are aligned correctly for whichever train is making its way through that section.

Small Layout Operations, Continued

We use a dispatcher to control all of the operational movement on the layout and he is kept busy issuing verbal track block warrants and keeping everyone on schedule and adjusting meets as the schedule slips. Because there are few long runs on the layout, requests come at the dispatcher constantly and it can become a challenge to keep the railroad operating smoothly. Add to that, constant whistle blowing for grade crossing, engine direction signals, bell ringing and steam sounds all around and it can be a little intense for the dispatcher.



Our scheduled layout distances are HO scale (87:1) plus a 10:1 compression which seems like a lot but works out just fine. Typical train average speeds are scheduled at 6-8 mph which may seem slow but fit our mostly mountain locos (Shays, Climaxes, Heislars) and small rod locos fairly protypically. They don't seem to be running slowly during operations but, of course, we are not going very far and who's in a hurry? Our passenger trains run a little faster and that keeps all of us hopping to keep the mains open on schedule for their passing. Train schedule grids become a challenge in that, due to the nature of the layout, it is almost impossible for a train to return to its base or staging using the same route it took to reach its furthest destination on an out and back schedule. Therefore, the distances and times taken to travel out and back are usually different. This adds another dimension to the dispatcher's job.

G&SRR Schedule Grid: Note the different distances for differing out and back routing. The letters indicate route blocks to be traversed.

Small Layout Operations, Continued

The reason for putting these operation sessions together in the first place is really two-fold. Initially talking with a small group of our NMRA model train friends it became clear that getting together a couple of times a month would be a nice break from the Covid restrictions. All of our friend members were double vaccinated plus vaccine boosters so we all felt comfortable being together. This social side of the reason for getting together has become something we all look forward to. We get together an hour or so in the evening before the scheduled operation start time for a Pizza dinner and train show-and-tell and conversation. We call the evening sessions “Pizza and Trains”. The second reason for putting these op sessions together is to forward all of our progress through the required operational time to qualify for our NMRA Achievement Program “Chief Dispatcher” Achievement Award while we are having our train fun.

In order to ensure that all of us in the group would be advancing toward the Dispatcher AP, required some sort of organization and we came up with the following scheme. We have defined four basic railroad positions for the group: Railroad Owner, Railroad Superintendent, Dispatcher and Train Operator (Engineer, yardmaster, etc.).

Railroad Owner: The primary job of the railroad (layout) owner (me) is to keep the layout and rolling stock in operational condition. Between operation sessions Bad Order cars and locos are repaired and track and power issues are resolved. I also take on the responsibility of cleaning the track before each operating session. Ah yes, one big advantage of operations on a small layout- I can clean all of the track on the layout in less than an hour!



Pat Raymer throttles the short log run down the grade from the KJ Lumber site.



Mona Raymer is sorting cars for the #4001E interchange freight due shortly in the Tesoro Yard.

Railroad Superintendent: The function of the superintendent in our group is to define 4 to 5 trains to be run simultaneously including schedules, switch lists, staging, cars and locos. Each op-session we rotate our operators through these different trains so that we all have a chance to run each of these trains over the course of 4-5 op-sessions. After rotating through these trains, one of our group members becomes the new railroad superintendent who is responsible for bringing a new set of trains to be run. For us, this happens every couple of months. The fresh set of trains keeps it interesting in that a train operator will rarely ever operate exactly the same train throughout our entire AP advancement program.

Small Layout Operations, Continued

Dispatcher: As the term implies, the dispatcher runs the railroad for the op session. He is responsible for keeping all of the trains on schedule, organizing and directing train meets, issuing track block warrants, departure clearances, etc. He is responsible for resolving schedule conflicts when trains are running late and issuing schedule modifications as required to keep things moving. This is all done verbally for our sessions (he is not far away). It is up to the session dispatcher to stage all of the locos and cars for the session and he is expected to show up a little early for the session to get this done. This position is part of our normal train operator rotation and because of that, all of us will act as dispatcher at some point for each set of superintendent defined trains. When a new superintendent introduces a new set of trains to the operating group, by default, he assumes the dispatcher position for that first op-session giving him the opportunity to see how things are running with his trains and giving him an opportunity to modify the schedules if needed, before the next session which will be run under a new dispatcher.

Train Operator: Paper Form 19 train orders / switch lists and Form A routing instructions are issued at the beginning of each session and the train operators are responsible for completing the tasks outlined. Special instructions can be issued with the paperwork or verbally by the dispatcher as the session progresses. The operator is responsible for prototypical operation of his loco to include the appropriate loco direction signals, grade and tunnel signals, bell operation etc. All turnouts (all 43 of them on the G&S) are electronically directed by panel toggle switches and it is up to the train operator to align them for his route and leave them directed to the main after passing. His goal is to complete his tasks on schedule according to the fast clock as best he can and to try not to overrun turnouts backwards. This in itself can be a challenge on a smaller layout. Yardmaster and towerman positions fall under this Train Operator category for our sessions.

G&S Railroad

FORM 19

Conductor/Engineer

Train: #1025 W KJ Lumber

From: G&S Maintenance Yard

To: Tsoro Station / KJ Lumber

Secure any Special Instructions for today's date-----
See G&S Yardmaster for Loco and Caboose Assign.-----
Tsoro Yard

Pull coach

Tsoro Station

Passenger Loading

Chip's Lumber

Pull (3) empty log bogies

Chantey Springs

Passenger Loading

KJ Lumber

Spot (3) empty log bogies

Prep Loco for KJL #1026 E

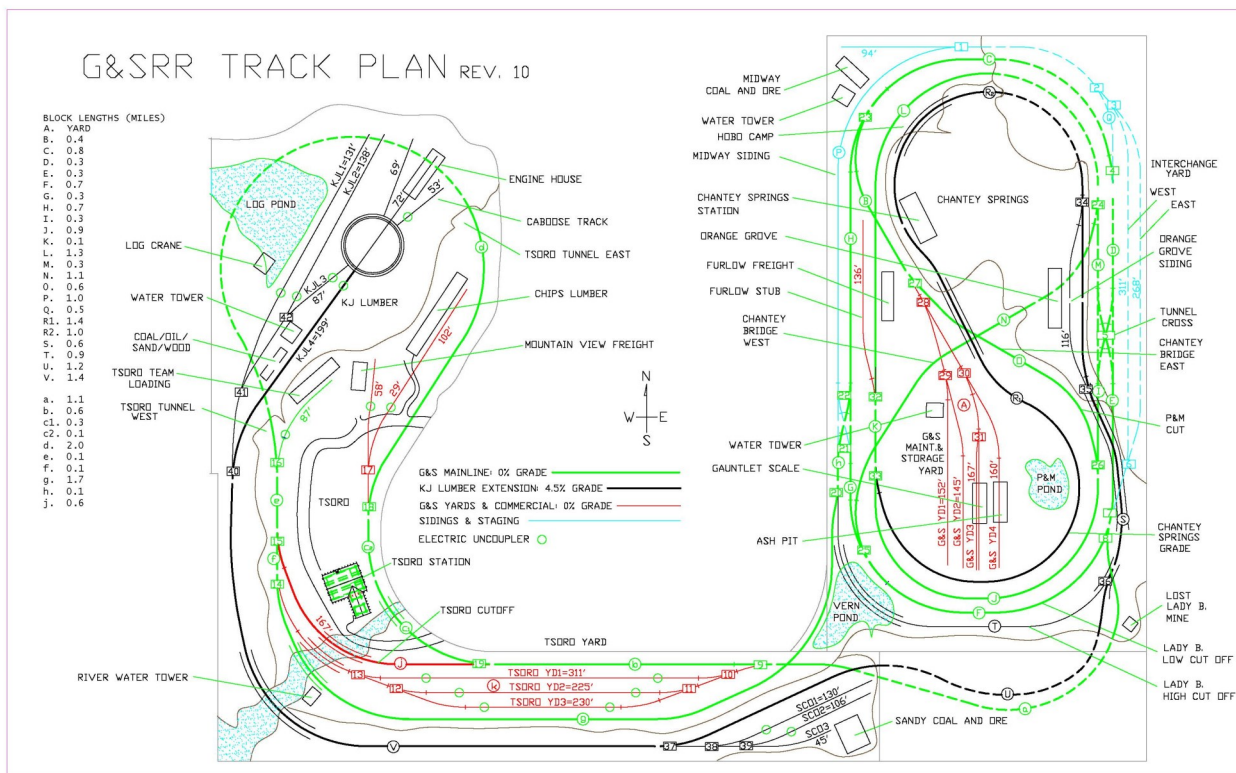
Park Loco for lunch clear of TT leads

Maintain a clear Main whenever possible and at all times specified by schedule or special instructions.

Gary Stenberg works the weekly #1003W passenger on its trip back down from the KJ Lumber facility.

We have found that operating sessions can definitely be held on smaller layouts and they can be a lot of fun. Our sessions may lapse into less than strictly formal at times, especially when someone overruns a turnout and the session on that part of the railroad comes to an electrical short halt or the group crowds together in one corner of the layout requiring another operator to throw a turnout at a panel out of reach or a train becomes lost in a tunnel somewhere, but all of it contributes to the experience.

I think we all have gained respect for how much work it takes to organize prototypical operation and the responsibilities that come with the dispatcher's job on a 1:1 railroad and I think that is at least one of the goals of the NMRA's Chief Dispatcher Achievement Award.



G&S Track Plan. I can supply this in .dwg format if desired.



Make A Wish Comes Through for a young model train enthusiast.

Make-A-Wish San Diego heard from 10-year old Beau who asked for his own DCC HO train layout. As a gift to Beau, Make-A-Wish San Diego contacted Lloyd's Layout's of Paramount California to build a great layout for him.

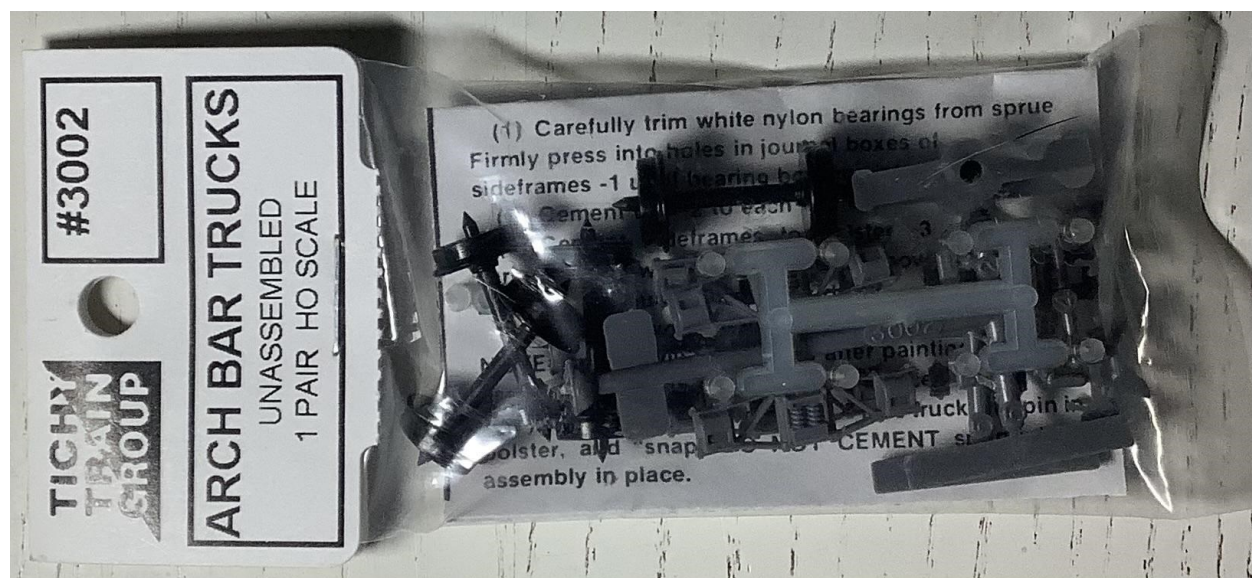
On December 7th at 9:30 PM the layout was presented to an excited young layout owner. The event was covered by KUSI TV, KGTV ABC 10, FOX 5 TV and NBC 7 at the San Diego Model Railroad Museum in Balboa Park very near the World Famous San Diego ZOO. SiriusXM "60s Gold" on channel 73 Radio and Television personality Shotgun Tom Kelly, an NMRA member, helped with the presentation of the Layout to Beau.

Lloyd and Betsy Kluesner of Lloyd's Layout's were Thrilled with Beau's reaction when he said "thank you" on TV as they were being interviewed on NBC 7, telling the story of Beau's new layout. His "thank you" touched everyone's hearts. (Article created from notes from Tom Irwin.—Ed.)



Fun With Tichy Arch Bar Trucks

By Michael James Starkey



I really like the proportions of Tichy's Arch Bar Trucks... except for the journal boxes. They include plastic wheelsets, but are too narrow for replacement Kadee wheelsets. With some .005 styrene I was able to solve both problems.

I started by cutting a strip of styrene $\frac{1}{8}$ " wide. This was trimmed into rectangles that were cemented to the end of each bolster. The extra .005 in width added to each side allows Kadee wheelsets to fit properly.

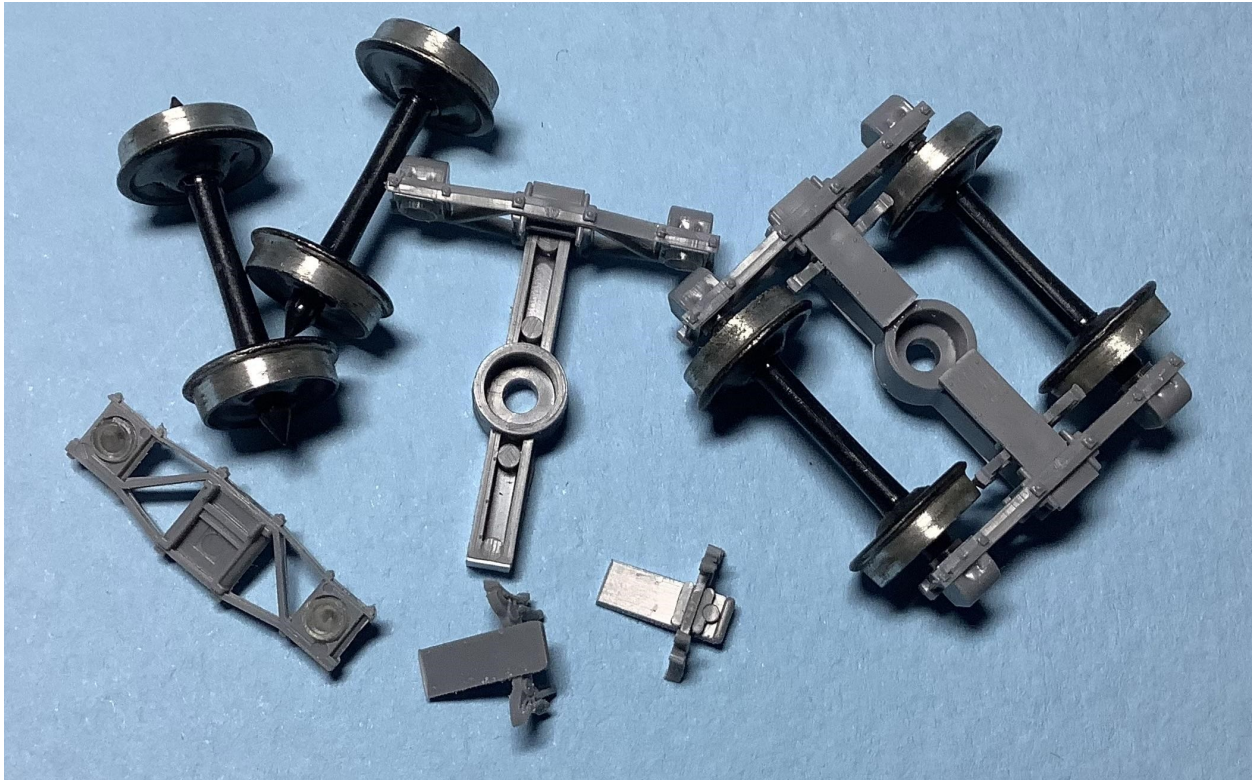
Next, I cut the $\frac{1}{8}$ " strip into 10" sections. These are the new journal box covers. A tiny triangle was trimmed from each bottom corner. More .005 styrene was cut into 3"x4" pieces to serve as hinges on each journal box.



Tichy, Kadee, and .005 styrene parts for two trucks.

Assembly mostly follows the directions. I used a number 11 blade and a fine cutter to remove a fair amount of flash from the side frames. The nylon bearings were inserted into the side frames; I added a little ACC. A bolster was cemented to a side frame. After it set up, wheelsets were inserted and the other bolster cemented in place.

Spring planks were trimmed to 2 ½ scale feet on each side to allow access to truck screws. Brake shoes castings were cemented onto each side, and I used ACC to hold them in place.



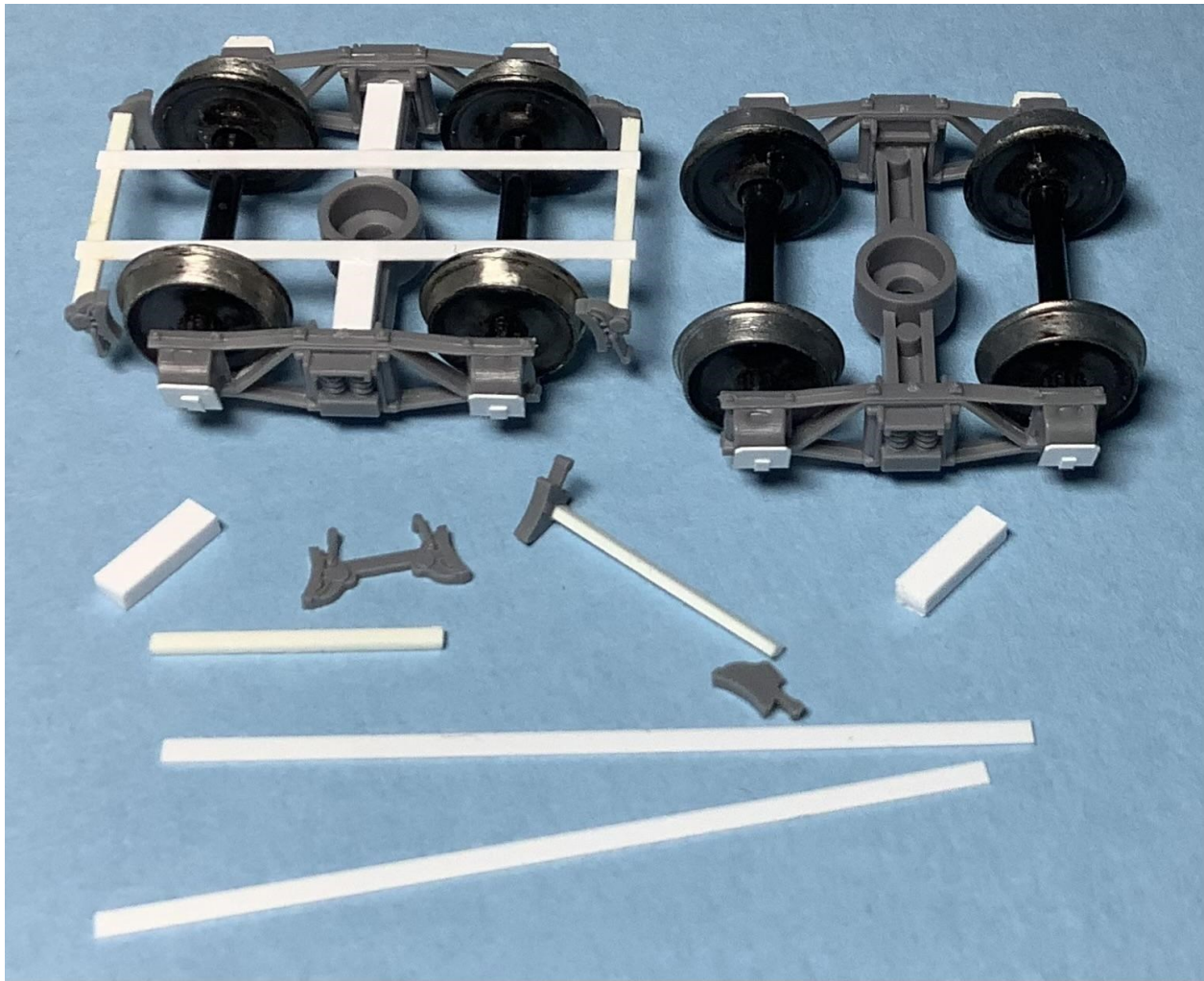
Truck assembly in progress on the left, and finished on the right.

Hinges were then cemented to the journal box covers, and the covers were ACC'd to the side frames.



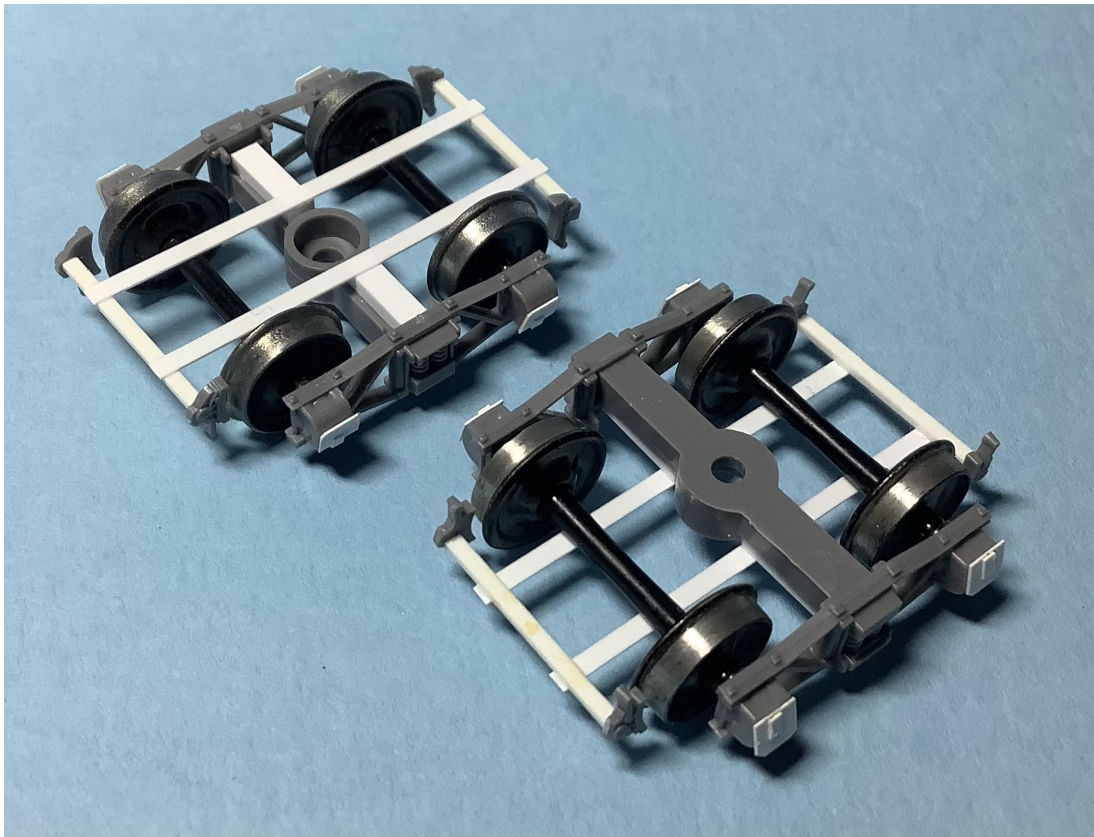
The finished trucks, ready for painting and installation.

As finished above, the trucks could be used from about 1890 through the 1920's. I also modified some trucks to replicate body hung brakes of the 1880's.



Underside of a truck showing the finished body hung brakes on the left, and the parts needed to complete a second truck.

I cemented the replacement spring planks in place, then the long strips that will support the brake beams. I was careful to keep them parallel. While they set up, I cut each brake shoe off the casting and used medium ACC to secure them one at a time to the brake beam. I tried to keep the same angle they have on the casting, but it's difficult to match them up exactly. When dry, I set them on the supports and cemented them in place. The last step was to trim the support just a bit past the brake beam.



Finished view of the underside and from the top for reference.



Ready for paint and installation.

There you have it: two ways to have fun with Tichy Arch Bar Trucks. With the addition of Kadee wheels, some styrene, and a little bit of work, these attractive trucks will look and run great under your rolling stock.

H0
SCALE

January 21, 2024

11:00am ~ Round Trip Pass ~ 5:00pm

RON VARNELL'S UNION PACIFIC LAYOUT

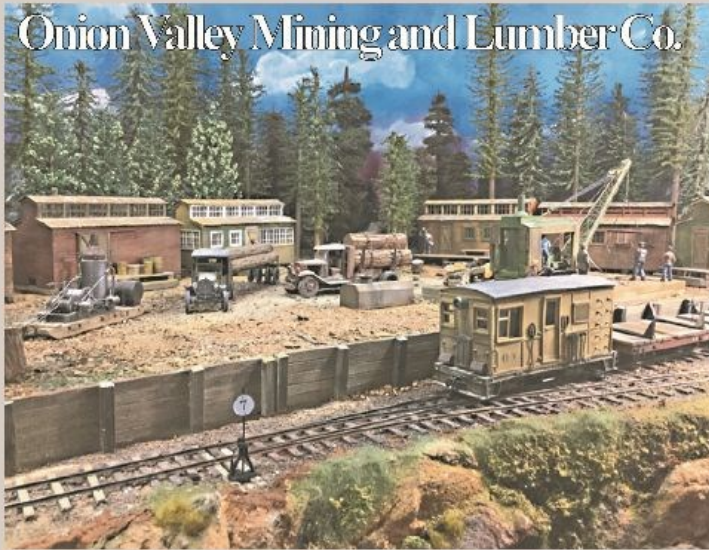


Set between 1958 and 1979, this 1,400 square foot layout features both standard and narrow gauge. It features logging, oil extraction, strip and gold mining, plus numerous other rail served industries. Join your fellow NMRA members for hot dogs, camaraderie, and a great model railroad!

2741 Plaza Del Amo, Suite 213, Torrance, Ca 90503

2024 JANUARY 13 2024
GREAT EVENT
GRAND RAILROAD TOUR
DAVID BALSER'S ON30

Union Valley Mining and Lumber Co.



ALL TRAINS LEAVE
Hemet, CA STATION
470 Garcia Drive

SCHEDULE
10 a.m. Depart ~ 2 p.m. Return

EXCLUSIVE FARE
FREE to NMRA Members & Guests

Show & Tell Bring your latest Project to Share

Notice! This Railroad is in the **FOUR SEASONS Gated Community in Hemet**. To insure access, make your reservations at least 2 days in advance
 email: On30man@Gmail.com
 or direct by phone: David / 760 436-6139

Lunch Will be Served!

As Usual, Expert Modeling Advice Available
 -We've got all kinds of Arm-Chair Experts-

I want to wish each of you and your families the best Holiday possible! Times are difficult for many, and we must remember the blessings each of us have each day. I wish for you that this Holiday Season, however you choose to celebrate it, be blessed and full of joy, love, happiness and everything that gives your hearts peace.

Kevin
 Spady—
 Editor



NMRA YouTube Channel

If you have access to the internet take a minute to check out the NMRA YouTube Channel at

<https://www.youtube.com/playlist?list=PLBbDHVLodgxn8otpnudMsHdQMucCzcTWD>

While it is challenging to watch videos live during the week if you are a working person, these video recordings of the presentations give you access to many events you might not otherwise get to see.



The 2024 NMRA National Convention
 "Surfliner 2024"
 will be in Long Beach from August 3 to 11, 2024

Did you know that the Pacific Southwest Region will be hosting the National NMRA Convention in Long Beach in 2024? Do you have a layout that you would like to share during the convention layout tours?

WE NEED LAYOUTS FOR THE LAYOUT TOURS!!!!!!!

If you would like your layout to be on the tour or, if you are building a layout and think it will be tour ready by August of 2024, please contact Tom Selinske at layouttours2024@gmail.com, or Morrie Fleishman at mefleishman@cox.net (949-351-7715) to get your layout added to our layout list. We will be reviewing all potential layouts over the next year to develop our layout tours and are looking for those awesome layouts that are hiding away in your basement or spare room! Detailed requirements for inclusion in the layout tours will be coming out in the next couple months. This is a first call for layouts and there will be plenty of time to finalize your participation over the next year.

This convention is attended by a huge audience from around the globe and this is a chance to show off your modeling skills and railroad creations! In addition to the recognition of your peers, your layout may qualify for an NMRA Golden Spike award and can earn credit for other NMRA MMR Achievement Awards.

If you, or anyone you know, may be interested, please have them contact Tom or Morrie.



Join us in the heart of Silicon Valley for The Tech Line, PCR Convention 2024



The 2024 PCR convention will be held April 24-28, 2024, at the Sonesta San Jose hotel in the middle of the golden triangle in Silicon Valley. Room rates are only \$139 per night.



Amazing Layout Tours

The layout tours will feature some of the most recognizable layouts in the country as well as some new entrants. We currently have over 20 layouts committed to be open and expect have at least 30 open by the time of the convention.



Bob Brown
Tuolumne Forks



Dave Adams
D&RGW Durlin Branch



Ed Loizeaux
New York Central



Seth Neumann
UP Oakland Sub

The Best Clinics

Learn from some of the best modelers in the country at an array of clinics. There will be a Tech Line focus on new technologies in model railroading.

Daily Special Event Options

Clinics will be in the Morning and Evenings with the afternoons held open for special activities;

- ✓ Layout Tours
- ✓ Vendor Room
- ✓ Modular Layout in the Hotel
- ✓ Special Clinics
- ✓ Swap Meet
- ✓ Prototype Tours
- ✓ 3D Printing Demos

Don't miss this great opportunity to experience all the Bay Area has to offer for Model Railroading.

**April 24-28
2024**

www.pcr2024.com



Kevin Spady, AIA, Editor
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Glendale, CA. 91206

